

PEORIA INNOVATION CORE (THE "PIC")

PCD Standards & Guidelines Report
(Case Z26-01)



Kimley»»Horn

PEORIA
INNOVATION
CORE



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Table of Contents

- 1. INTRODUCTION 1**
- 2. PROPERTY DESCRIPTION 1**
- 3. PRELIMINARY DEVELOPMENT PLAN 1**
 - 3.1. Development Concept..... 3
 - 3.2. Cores & Development Parcels 3
 - 3.3. Land Use Designations 3
 - 3.4. Permitted, Conditional, and Accessory Uses 13
 - 3.5. Development Standards..... 13
 - 3.6. Loop 303 Height Overlay..... 15
 - 3.7. Parking Standards..... 15
 - 3.8. Accessory Buildings and Uses 15
 - 3.9. Accessory Dwelling Units 15
 - 3.10. Open Space Requirements 16
 - 3.11. Lighting..... 16
 - 3.12. Screening, Fencing and Walls..... 16
 - 3.13. Roadway Standards 16
 - 3.14. Design Review Standards 17
 - 3.15. Landscaping 17
 - 3.16. Signs 17
- 4. PROJECT IMPLEMENTATION 17**
 - 4.1. Project Governance..... 17
 - 4.2. Master and Secondary Planning 17
 - 4.3. Development Parcel Plan Requirements 18
 - 4.4. Vehicular Circulation: 18
 - 4.5. Drainage:..... 18
 - 4.6. Water:..... 18
 - 4.7. Wastewater: 18
 - 4.8. Open Space, Recreation & Trails:..... 21
 - 4.9. Public Facilities:..... 21
 - 4.10. Unspecified Land Uses 21
 - 4.11. Development Regulations and Standards Not Listed..... 21
 - 4.12. ASLD/Purchaser Role 21
 - 4.13. Land Use Budget and Planning Allocation 22
 - 4.14. Amendments 22



5. INFRASTRUCTURE 22

5.1. Street Circulation..... 22

5.2. Water and Wastewater..... 22

5.3. Drainage..... 22

5.4. Electrical Power Demand/Transmission..... 22

List of Tables

TABLE 1A: LAND USE DESIGNATIONS 9

TABLE 1B: BASE LAND USES 9

TABLE 2: PERMITTED, CONDITIONAL & ACCESSORY USES 11

TABLE 3A: SUPPLEMENTAL USES..... 13

TABLE 3B: PROHIBITED USES..... 13

TABLE 4A & 4 B: DEVELOPMENT STANDARDS 14

List of Exhibits

EXHIBIT 1 - VICINITY MAP 2

EXHIBIT 2 - CORE MAP 4

EXHIBIT 3 - LAND USE PLAN 5

EXHIBIT 3A - CORE 1 LAND USE PLAN..... 6

EXHIBIT 3B - CORE 2 LAND USE PLAN..... 7

EXHIBIT 3C - CORE 3 LAND USE PLAN..... 8

EXHIBIT 4 - STREET CIRCULATION PLAN..... 19

EXHIBIT 5 - PATH AND TRAILS 20

EXHIBIT 6 - LAND USE BUDGET 23

List of Appendices

APPENDIX A: LEGAL DESCRIPTIONS 25



1. Introduction

The Peoria Innovation Core (“PIC” or “Project”) comprises approximately 7,341 acres of predominantly State Trust Land located within the northeastern portion of the City of Peoria, Arizona. In coordination with the Arizona State Land Department (“ASLD”), Amkor Technology and the Roman Catholic Diocese, the City of Peoria is initiating a request to rezone the property to the Planned Community District (“PCD”) designation. The purpose of this request is to establish a comprehensive planning framework that supports the City’s community and economic development objectives while facilitating the responsible, long-term buildout of the area.

The PCD Zoning District will provide the regulatory foundation for future development through the establishment of base zoning, conceptual infrastructure planning, and detailed design standards and guidelines. The structure of the PCD as a “zoning bank” reflects the unique mission of ASLD by allowing flexibility to respond to evolving market conditions, while ensuring that development remains consistent with the City’s vision for balanced, high-quality growth.

The Project’s location along key regional transportation corridors, including Loop 303, SR-74, and Lake Pleasant Parkway, positions it as a strategic opportunity for the expansion of employment, commercial, hospitality, and a mix of housing types. Proximity to existing infrastructure, regional employers, and major roadways further supports the viability of this area for future development.

The proposal is consistent with the City’s PlanPeoriaAZ 2040 General Plan and ASLD’s Special Land Use classification, both of which emphasize flexible, mixed-use development to accommodate population growth, promote economic vitality, and expand local employment opportunities. The 7,341-acre site is organized into three (3) primary development cores, allowing for distinct land use strategies and infrastructure planning tailored to each area’s physical characteristics.

2. Property Description

As described in *Appendix A – Legal Descriptions*, the Peoria Innovation Core (PIC) is generally located in Sections 33 and 34 of Township 06 North Range 01 East, and portions of Sections 1 through 28 of Township 05 North Range 01 East. As shown in *Exhibit 1 - Vicinity Map*, the site is bisected from north to south by the Loop 303 and the Central Arizona Project (“CAP”) Canal, and from east to west by Lake Pleasant Parkway and the Waddell Canal. There are transmission line corridors associated with the Morgan and Raceway substations that bisect the property near the western boundary as well. Additionally, the Kinder-Morgan gas line and associated easement bisect the property along the bluff area west of Lake Pleasant Parkway.

From a regional perspective, the Peoria Innovation Core is uniquely situated within close proximity of regional drivers including the Lake Pleasant Regional Park, Peoria Regional Preserve, and large-scale employers such as the Taiwan Semiconductor Manufacturing Company (“TSMC”), the largest construction project in the United States and the largest jobs creator within the region, and the emerging Amkor Technology within the PIC. Other surrounding land uses include existing residential developments Westwing Mountain and Pleasant Valley to the south and open space, defined by the southern flow of the Agua Fria River, with the emerging master planned communities of Lake Pleasant Heights and Saddleback Heights beyond that to the west.

3. Preliminary Development Plan

The intent of this application is to establish an entitlement for the property that will advance community goals and priorities articulated in the Peoria General Plan while providing for flexibility to respond to market conditions. Since there is no designated master developer for the entire Project, the use of the PCD zoning district is intended to create a development framework that will position the property for future market readiness.

ARIZONA 74

PEORIA INNOVATION CORE OVERALL BOUNDARY

PEORIA CITY LIMITS PHOENIX CITY LIMITS

ARIZONA 74

OLD CAREFREE HWY.

DOVE VALLEY RD.

LOOP 303

UNINCORP. MARICOPA COUNTY

AMKOR WY.

ARIZONA STATE LAND DEPARTMENT

LOOP 303

MONTGOMERY RD.

FLOOD CONTROL DISTRICT OF MARICOPA COUNTY

DIXILETA DR.

PALOMA COMMUNITY PARK

96TH AVE.

LONE MTN. PKWY. LAKE PLEASANT PKWY.

WESTWING PKWY.

TIERRA DEL RIO NORTH

99TH AVE.

PLEASANT VALLEY

CIBOLA VISTA

WESTWING MOUNTAIN

ROCK SPRINGS

LEGEND

- PIC OVERALL BOUNDARY
- CORE BOUNDARIES
- RIGHT-OF-WAY/CANAL
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT





3.1. Development Concept

The Planned Community District (PCD) zoning provides greater flexibility compared to traditional zoning regulations, promoting more efficient and appropriate use of both land and infrastructure.

This adaptable approach is particularly suited to the Arizona State Land Department (ASLD), enabling development that can respond effectively to changing market conditions and surrounding land uses. The proposed Plan supports several policies outlined in the General Plan by offering a variety of land uses aligned with the City's objectives for growth and job creation. This alignment forms the basis for future master planning and site-specific development.

The PCD framework guides the coordinated integration of residential and non-residential land uses near the Loop 303 Freeway corridor. It upholds the City of Peoria's General Plan vision by "promoting employment and commercial development adjacent to the freeway, with appropriate transitions to residential areas and supporting services that reflect the land use patterns of neighboring communities".

Unless explicitly stated within the PCD, all development must adhere to the applicable provisions of the City of Peoria Zoning Ordinance and Community Design Guidelines.

3.2. Cores & Development Parcels

Given the expansive nature of the Project and the physical constraints within and around the PIC, the larger area is divided into three (3) Core areas as shown in *Exhibit 2 - Core Map*. Within the boundaries of the Core areas, each is then further delineated into sub-areas or Development Parcels, which are primarily defined by existing physical features such as arterial and collector roads, the Loop 303 freeway, Canals, natural drainage areas, and established utility areas. These delineation efforts help inform how and when portions of the PIC are ultimately brought forth for auction.

Each Development Parcel is defined by factors such as land use characteristics, infrastructure service, and natural or constructed boundaries. Each parcel is

assigned a specific land use designation, which may correspond to one or more zoning districts recognized by the City of Peoria, or custom districts tailored for this PCD, governed by the density and use allowances associated with the zoning designations listed in *Table 2: Permitted, Conditional and Accessory Uses*. Individual Development Parcels may accommodate one or several development projects or site plans. Refer to *Exhibit 3 – Land Use Plan* for additional detail.

To allow flexibility in meeting market conditions and community needs, dwelling units may be transferred between Development Parcels, provided they remain within the same designated water and wastewater service area. Such transfers are permitted as long as the overall unit cap established for the PCD is not exceeded. Under no circumstances may units be transferred from areas outside of the PCD into areas within it.

3.3. Land Use Designations

This Planned Community District (PCD) assigns tailored land use designations to each of the delineated parcels within the Core Area. In many cases, multiple land uses have been conjoined and applied to individual parcels to promote flexibility in future development and allow responsiveness to evolving market demands. The PCD sets forth a comprehensive land use framework, or "land use budget," which ensures that this range of uses can be implemented in a compatible manner.

Parcel-specific land use designations are depicted in *Exhibit 3 - Land Use Plan* and shown in greater detail within *Exhibits 3A - 3C*. Unless otherwise modified by this PCD, development must comply with the standards outlined in the Peoria zoning districts assigned to each parcel, as specified in *Table 2: Permitted, Conditional and Accessory Uses*. Consistency with the City's existing residential and non-residential zoning districts provides continuity with established land use permissions and development standards set forth in the Zoning Ordinance. Any deviations from standard zoning regulations are detailed in *Table 4A & 4B: Development Standards*.

ARIZONA 74

PEORIA INNOVATION CORE OVERALL BOUNDARY

PEORIA CITY LIMITS PHOENIX CITY LIMITS

ARIZONA 74

ARIZONA STATE LAND DEPARTMENT

RACEWAY SUBSTATION

OLD CAREFREE HWY.

CORE 3

DOVE VALLEY RD.

LOOP 303

UNINCORP. MARICOPA COUNTY

CORE 2

ARIZONA STATE LAND DEPARTMENT

BUREAU OF LAND MANAGEMENT (BLM)

CORE 1

FLOOD CONTROL DISTRICT OF MARICOPA COUNTY

LOOP 303

ARIZONA STATE LAND DEPARTMENT

SONORAN MOUNTAIN RANCH

LEGEND

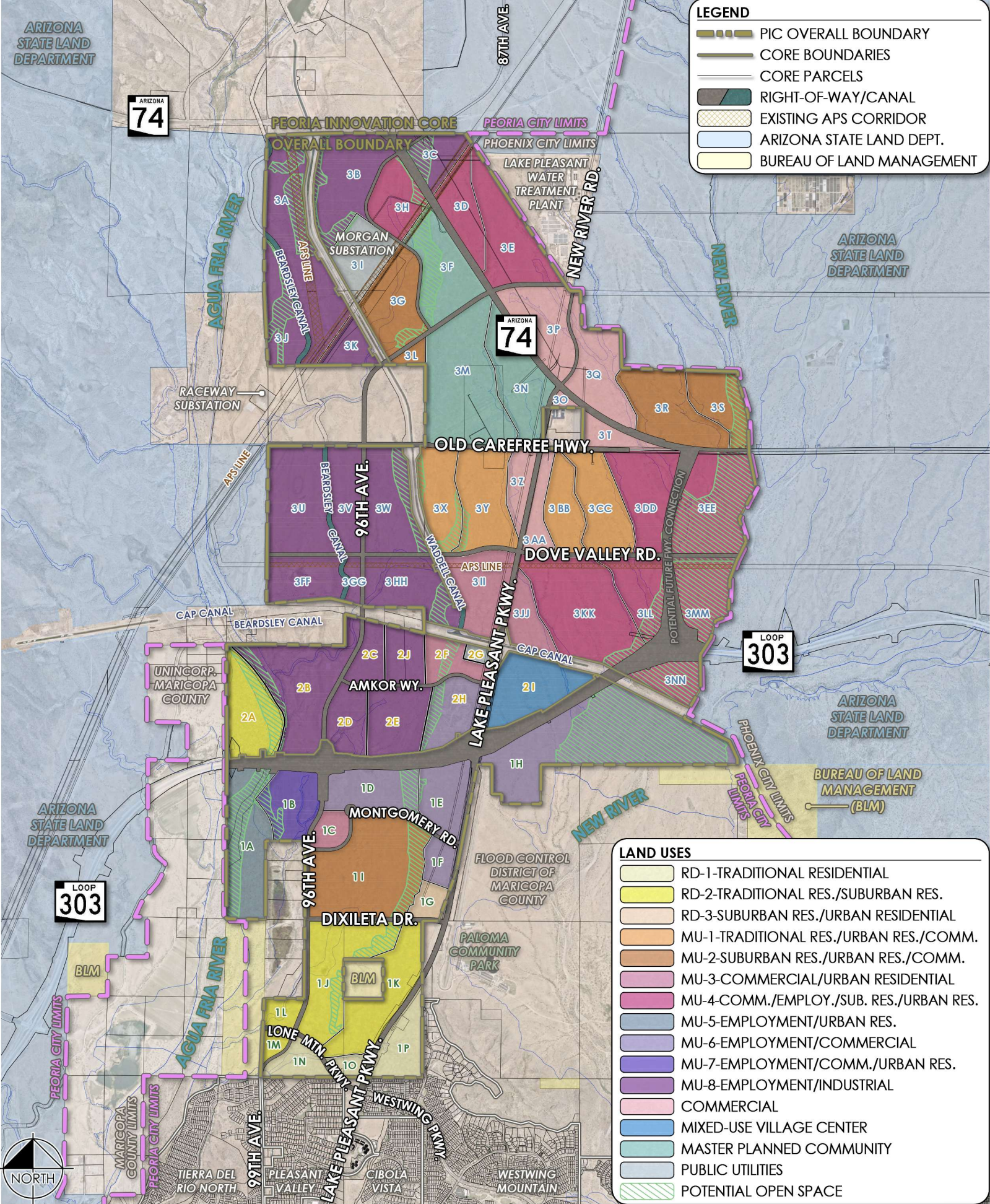
- PIC OVERALL BOUNDARY
- CORE BOUNDARIES
- RIGHT-OF-WAY/CANAL
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT



ARIZONA 74

LEGEND

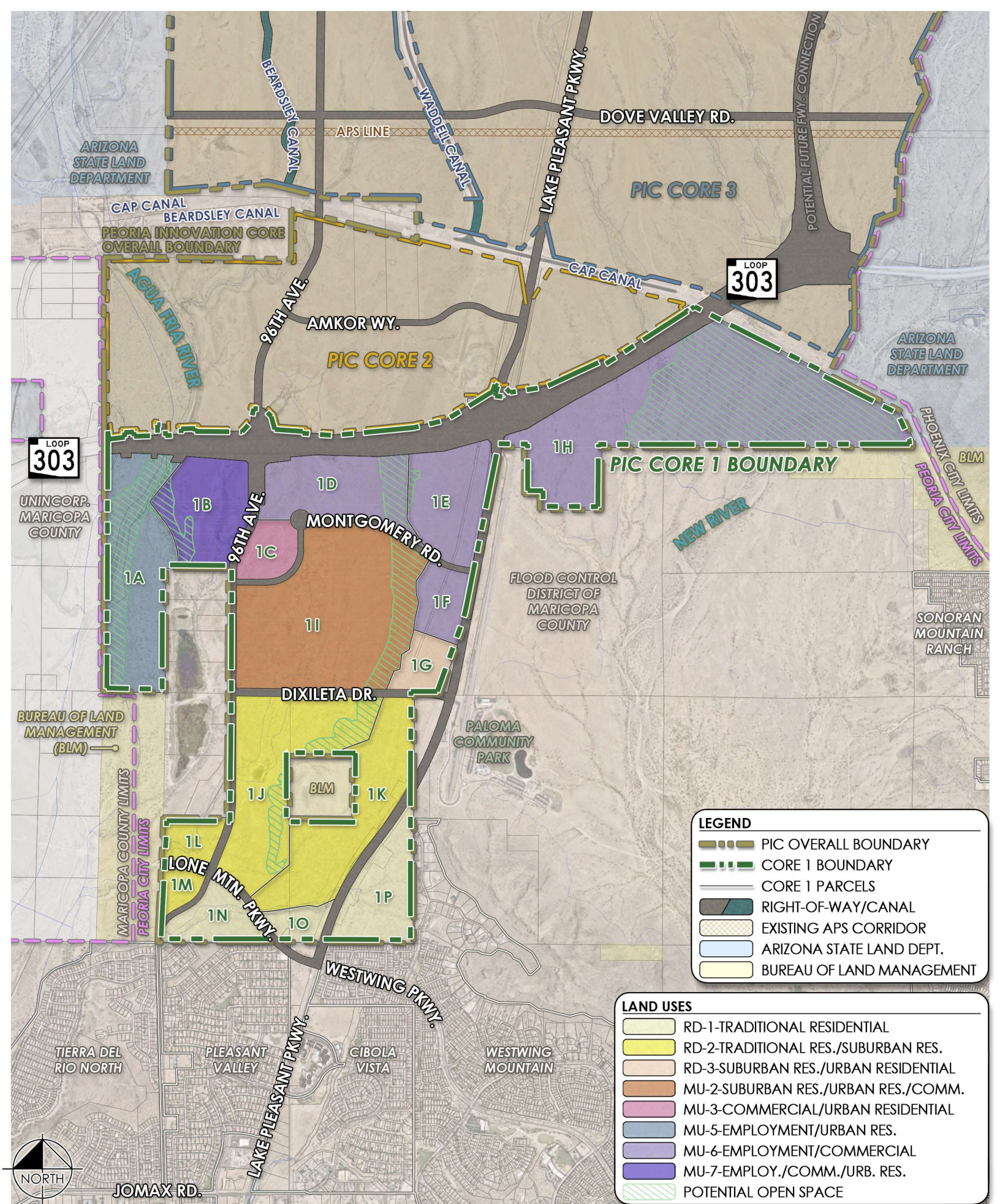
- PIC OVERALL BOUNDARY
- CORE BOUNDARIES
- CORE PARCELS
- RIGHT-OF-WAY/CANAL
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT



LAND USES

- RD-1-TRADITIONAL RESIDENTIAL
- RD-2-TRADITIONAL RES./SUBURBAN RES.
- RD-3-SUBURBAN RES./URBAN RESIDENTIAL
- MU-1-TRADITIONAL RES./URBAN RES./COMM.
- MU-2-SUBURBAN RES./URBAN RES./COMM.
- MU-3-COMMERCIAL/URBAN RESIDENTIAL
- MU-4-COMM./EMPLOY./SUB. RES./URBAN RES.
- MU-5-EMPLOYMENT/URBAN RES.
- MU-6-EMPLOYMENT/COMMERCIAL
- MU-7-EMPLOYMENT/COMM./URBAN RES.
- MU-8-EMPLOYMENT/INDUSTRIAL
- COMMERCIAL
- MIXED-USE VILLAGE CENTER
- MASTER PLANNED COMMUNITY
- PUBLIC UTILITIES
- POTENTIAL OPEN SPACE





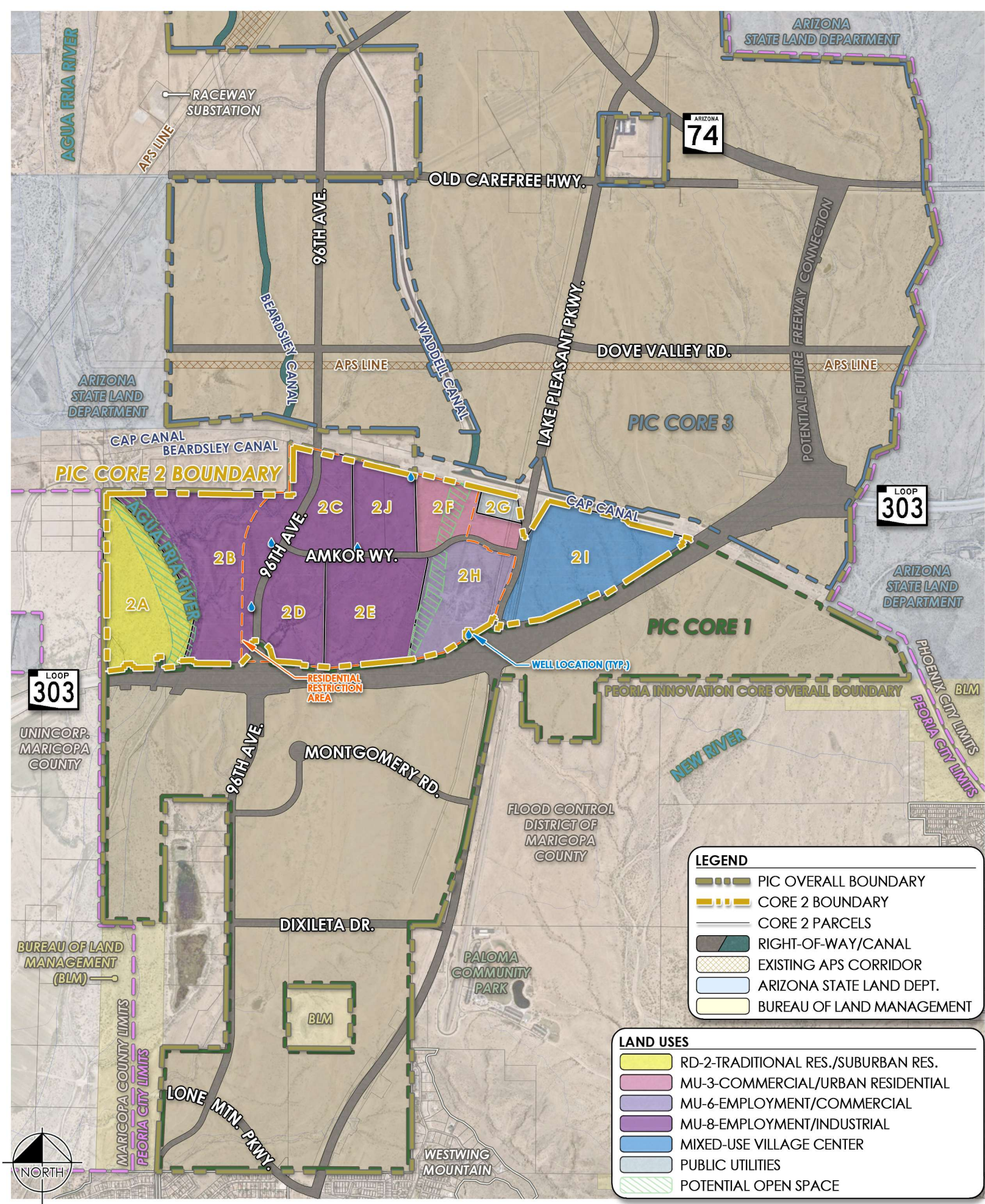
LEGEND

- PIC OVERALL BOUNDARY
- CORE 1 BOUNDARY
- CORE 1 PARCELS
- RIGHT-OF-WAY/CANAL
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT

LAND USES

- RD-1-TRADITIONAL RESIDENTIAL
- RD-2-TRADITIONAL RES./SUBURBAN RES.
- RD-3-SUBURBAN RES./URBAN RESIDENTIAL
- MU-2-SUBURBAN RES./URBAN RES./COMM.
- MU-3-COMMERCIAL/URBAN RESIDENTIAL
- MU-5-EMPLOYMENT/URBAN RES.
- MU-6-EMPLOYMENT/COMMERCIAL
- MU-7-EMPLOY./COMM./URB. RES.
- POTENTIAL OPEN SPACE





LEGEND	
	PIC OVERALL BOUNDARY
	CORE 2 BOUNDARY
	CORE 2 PARCELS
	RIGHT-OF-WAY/CANAL
	EXISTING APS CORRIDOR
	ARIZONA STATE LAND DEPT.
	BUREAU OF LAND MANAGEMENT

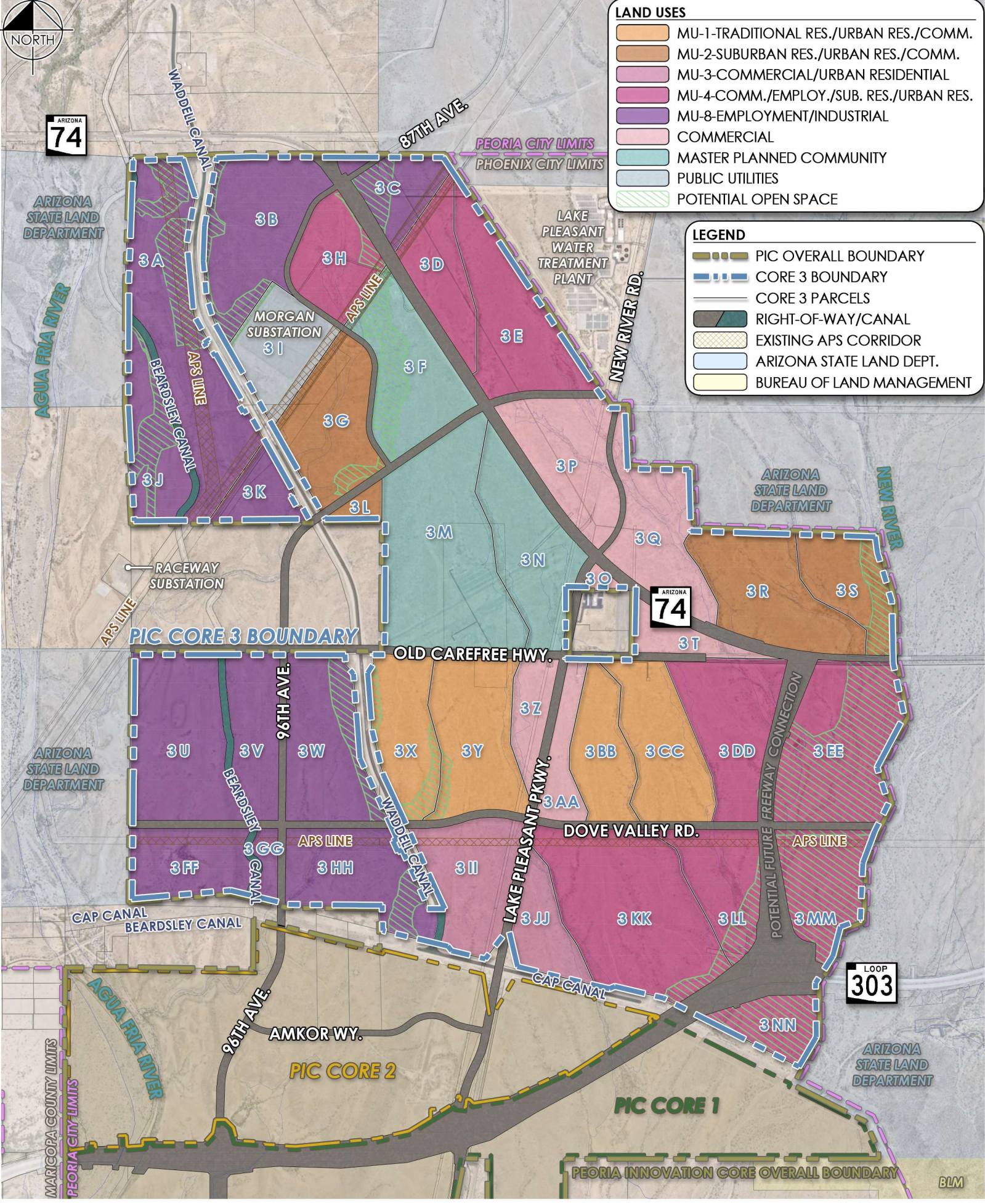
LAND USES	
	RD-2-TRADITIONAL RES./SUBURBAN RES.
	MU-3-COMMERCIAL/URBAN RESIDENTIAL
	MU-6-EMPLOYMENT/COMMERCIAL
	MU-8-EMPLOYMENT/INDUSTRIAL
	MIXED-USE VILLAGE CENTER
	PUBLIC UTILITIES
	POTENTIAL OPEN SPACE



ARIZONA
74

LAND USES	
	MU-1-TRADITIONAL RES./URBAN RES./COMM.
	MU-2-SUBURBAN RES./URBAN RES./COMM.
	MU-3-COMMERCIAL/URBAN RESIDENTIAL
	MU-4-COMM./EMPLOY./SUB. RES./URBAN RES.
	MU-8-EMPLOYMENT/INDUSTRIAL
	COMMERCIAL
	MASTER PLANNED COMMUNITY
	PUBLIC UTILITIES
	POTENTIAL OPEN SPACE

LEGEND	
	PIC OVERALL BOUNDARY
	CORE 3 BOUNDARY
	CORE 3 PARCELS
	RIGHT-OF-WAY/CANAL
	EXISTING APS CORRIDOR
	ARIZONA STATE LAND DEPT.
	BUREAU OF LAND MANAGEMENT





It is important to note that while land use designations are applied to the entire extent of an individual parcel, this designation should not be construed to mean the total developable area for the parcel. Rather, the land use designation establishes the specific land use types, which then determines the applicable development standards for each parcel. The applicable development standards (e.g. setbacks, lot coverage, building height) then in turn establishes or sets the actual perimeter on the parcel where construction must fit within. Depending on the type of use, or whether the parcel is within an environmentally sensitive area, the actual extent of developable area may be further modified in conjunction with applicable city regulations.

The land use designations associated with the PIC include:

TABLE 1A: LAND USE DESIGNATIONS
Residential Development (RD)
RD-1: Traditional Residential
RD-2: Traditional Residential /Suburban Residential
RD-3: Suburban Residential /Urban Residential
Mixed Uses
MU-1: Traditional Residential / Urban Residential / Commercial
MU-2: Suburban Residential / Urban Residential / Commercial
MU-3: Commercial / Urban Residential
MU-4: Commercial / Employment / Suburban Residential / Urban Residential
MU-5: Employment / Urban Residential
MU-6: Employment / Commercial
MU-7: Employment / Commercial / Urban Residential
MU-8: Employment / Industrial

The overarching land use designations within the PIC include the following and have been applied to each parcel. The land use designations correlate with the available base zoning options pursuant to *Table 2*.

TABLE 1B: BASE LAND USES	CORRELATING BASE DISTRICTS
Traditional Residential	SF-1 (Traditional Single-Family Residential)
Suburban Residential	SF-2 (Small-lot Single-Family Residential), MF-1 (Detached Multi-Family Residential)
Urban Residential	MF-1, MF-2 (Traditional Multi-Family Residential)
Commercial	O-1, C-2, C-5
Mixed-Use Village Center	O-1, C-2, C-5, MF-1, MF-2, BPI
Employment	O-1, C-2, C-5, BPI
Master Planned Community	SF-1, SF-2, MF-1, MF-2, O-1, C-2, C-5
Industrial	BPI, I-1
Public Utilities (Public/Quasi-Public)	I-1

Traditional Residential

Traditional Residential (2-5 du/ac) designated properties are generally located in areas proximate to existing single-family residential uses. This land use type provides appropriate transitions along these “shared edges”. Residential types are limited to detached single-family residential neighborhoods.

Suburban Residential

Suburban Residential (5-12 du/ac) land use types represent a blend of single family and moderate density multi-family residential. These use types are generally intended to provide transitional development types between traditional residential and higher intensity parcels proximate to the Loop 303 Freeway corridor.

Urban Residential

Urban Residential (12+ du/ac) consists of range of ownership and rental housing options including townhomes, casitas, and other traditional types of multi-family.

Urban Residential areas enjoy a high level of access, and are located near services, employment centers and transportation corridors. Urban Residential is where the highest multi-family residential density



development is appropriate. The density range is intended to provide for multi-story apartments, condominiums, and townhouses which are close to employment and service areas. This category is traditionally located along transportation corridors and serves to buffer Suburban Residential areas from non-residential areas or Mixed-Use areas. Substantial common open space, recreational amenities and on-site support facilities serve the residents.

Commercial

Commercial areas typically include large-scale retail uses, grocery stores, specialty stores, and hospitality or entertainment uses. These areas have a wider variety of goods and services than neighborhood scale shopping areas. Located along arterial streets for ease of access, Commercial areas are typically auto-oriented developments that often feature internal circulation to get to anchor buildings or pad sites.

Mixed-Use Village Center

Mixed-Use Village Centers provide for a subregional market draw that supports an integrated mix of high-density residential products in a walkable format with an intensive mix of uses allowing for the integration of public land use types such as commercial, hospitality, office, employment with private high-density residential land uses. Mixed-Use Village Centers are stop and stay developments where employment is supported by contiguous residential units that are conveniently accessible by walking or biking. Residents and visitors are able to walk to retail, entertainment, public gathering spaces, and other office or commercial services. These areas easily cater to residents who cannot or prefer not to drive as a primary means of transportation, such as empty nesters or young professionals.

Employment

These areas provide employment opportunities that contribute to the overall local and regional economy. Employment areas are classified into two separate sub-types, Business Commerce and Industrial. Peoria strives to be a community that supports a culture of entrepreneurship. By allocating land within the city that allows for business expansion dedicated solely to

employment uses, supportive or complimentary uses may be collocated near each other for synergy in sharing of technology infrastructure and information. Business Commerce areas contain business offices, corporate headquarters, incubator space, research and development, and light industrial uses with complementary commercial uses within a campus format that features common landscaping and centralized access points. Site planning should balance siting of the building for visibility with appropriate screening of loading bays, truck access, screened storage areas, and employee parking.

Master Planned Community

Master-Planned Community provides for a large-scale, comprehensively designed development that strategically mixes residential, commercial, and recreational land uses to create a self-sustaining neighborhood. This designation is part of a long-term vision for growth, allowing for flexibility in development while ensuring the community has integrated amenities like parks, schools, and shopping centers for its residents.

Industrial

Industrial zones are designated for intensive industrial operations such as warehousing, manufacturing, processing, repairs, packaging, and the distribution of goods. Developments within industrial areas should incorporate enhanced architectural features on building facades that are visible from public streets, complemented by landscaped buffers. Additionally, the use of appropriate screening walls and designated areas for outdoor storage is encouraged—both to secure materials and to provide a visual buffer from nearby land uses.

Public Utilities (Public/Quasi-Public)

Public Utilities refers to areas designated for infrastructure like water and waste treatment plants, which serve the public. These are considered a "public" or "quasi-public" land use category and identify where major facilities for electricity, gas, communications, water, drainage, and sewage disposal will be located, ensuring that development is supported by necessary services.



TABLE 2: PERMITTED, CONDITIONAL & ACCESSORY USES

DEVELOPMENT AREA			ACRES			SINGLE-FAMILY RESIDENTIAL		MULTI-FAMILY RESIDENTIAL		NON- RESIDENTIAL					
CORE	MAX DU	PARCEL	GROSS	FINAL PLAT PARCEL DU	LAND USE	SF-1 TRADITIONAL SINGLE-FAMILY	SF-2 SMALL-LOT SINGLE-FAMILY	MF-1 DETACHED MULTI-FAMILY	MF-2 TRADITIONAL MULTI-FAMILY	O-1	C-2	C-5	BPI	I-1	
1	3,500	A	175.98		MU-5			X	X	X	X	X	X		
		B	113.24		MU-7			X	X	X	X	X	X		
		C	37.35		MU-3				X	X	X	X	X		
		D	149.03		MU-6						X	X	X	X	
		E	128.17		MU-6						X	X	X	X	
		F	39.53		MU-6						X	X	X	X	
		G	30.59		RD-3			X	X	X					
		H	417.18		MU-6						X	X	X	X	
		I	278.52		MU-2			X	X	X	X	X	X		
		J	180.40		RD-2		X	X	X						
		K	143.91		RD-2		X	X	X						
		L	30.25		RD-2		X	X	X						
		M	21.04		RD-2		X	X	X						
		N	48.44		RD-1		X								
		O	24.26		RD-1		X								
P	82.22		RD-1		X										
			1,900.12												
2	800	A	119.91		RD-2	X	X	X							
		B	200.55		MU-8					X	X	X	X	X	
		C	51.24		MU-8						X	X	X	X	X
		D	71.55		MU-8						X	X	X	X	X
		E	106.96		MU-8						X	X	X	X	X
		F	56.91		MU-3				X	X	X	X	X		
		G	10.77		PU										X
		H	88.80		MU-6						X	X	X	X	
		I	129.25		MUVC				X	X	X	X	X	X	
		J	59.26		MU-8						X	X	X	X	X
			895.21												
3		A	230.30		MU-8					X	X	X	X	X	
		B	160.61		MU-8					X	X	X	X	X	
		C	74.31		MU-8						X	X	X	X	
		D	84.58		MU-4		X		X	X	X	X	X	X	
		E	149.93		MU-4		X		X	X	X	X	X	X	
		F	145.80		MPC		X	X	X	X	X	X	X		
		G	123.22		MU-2		X	X	X	X	X	X	X		
		H	89.46		MU-4		X	X	X	X	X	X	X	X	
		I	86.30		PU										X
		J	96.52		MU-8						X	X	X	X	X



TABLE 2: PERMITTED, CONDITIONAL & ACCESSORY USES (CONT'D)

DEVELOPMENT AREA			ACRES			SINGLE-FAMILY RESIDENTIAL		MULTI-FAMILY RESIDENTIAL		NON- RESIDENTIAL					
CORE	MAX DU	PARCEL	GROSS	FINAL PLAT PARCEL DU	LAND USE	SF-1 TRADITIONAL SINGLE-FAMILY	SF-2 SMALL-LOT SINGLE-FAMILY	MF-1 DETACHED MULTI-FAMILY	MF-2 TRADITIONAL MULTI-FAMILY	O-1	C-2	C-5	BPI	I-1	
3 (CONT'D)	6,700	K	43.96		MU-8					X	X	X	X	X	
		L	16.90		MU-2		X		X	X	X	X			
		M	254.80		MPC		X	X	X	X	X	X	X		
		N	142.80		MPC		X	X	X	X	X	X	X		
		O	11.12		COM						X	X	X		
		P	131.23		COM						X	X	X		
		Q	129.90		COM						X	X	X		
		R	147.57		MU-2			X	X	X	X	X	X		
		S	84.64		MU-2			X	X	X	X	X	X		
		T	50.23		COM						X	X	X		
		U	181.48		MU-8						X	X	X	X	X
		V	94.71		MU-8						X	X	X	X	X
		W	144.61		MU-8						X	X	X	X	X
		X	98.31		MU-1		X	X	X		X	X	X		
		Y	143.56		MU-1		X	X	X		X	X	X		
		Z	50.20		COM						X	X	X		
		AA	55.82		COM						X	X	X		
		BB	82.35		MU-1		X	X	X		X	X	X		
		CC	123.08		MU-1		X	X	X		X	X	X		
		DD	146.05		MU-4			X	X	X	X	X	X	X	
EE	199.55		MU-4			X	X	X	X	X	X	X			
FF	89.62		MU-8						X	X	X	X	X		
GG	22.59		MU-8						X	X	X	X	X		
HH	122.98		MU-8						X	X	X	X	X		
II	115.12		MU-3					X	X	X	X				
JJ	62.49		MU-3					X	X	X	X				
KK	245.07		MU-4			X		X	X	X	X	X			
LL	110.75		MU-4			X		X	X	X	X	X			
MM	128.44		MU-4			X		X	X	X	X	X			
NN	74.84		MU-4			X		X	X	X	X	X			
TOTAL			4,545.80												

NOTES:

1. Areas with a base zoning district of Small-Lot Single-Family Residential (SF-2) shall adhere to Sections 21-416, 21-417 and 21-418 of the City of Peoria Zoning Ordinance governing Permitted, Conditional and Accessory Uses, but shall allow for at least one attached single-family dwelling per lot as a permitted principal use. Non-Residential uses within this base zoning district shall follow the property development standards for the R1-6 zoning district.
2. Areas with a base zoning district of Traditional Multi-Family Residential (MF-2) shall adhere to Section 21-425, 21-426 and 21-427 of the City of Peoria Zoning Ordinance governing Permitted, Conditional and Accessory Uses.
3. Areas with a base zoning district of Detached Multi-Family Residential (MF-1) shall allow uses in which multi-family units are detached from other units.
4. Residential uses shall be prohibited within the defined residential restriction area.



3.4. Permitted, Conditional, and Accessory Uses

Unless specifically modified by this PCD, all permitted, conditional, and accessory uses must adhere to the use regulations and restrictions outlined in the City of Peoria Zoning Ordinance for the zoning district identified on the Final Plat or Site Plan.

At the time of Site Plan submission (for non-residential or multifamily projects) or Preliminary Plat submission (for single-family residential development), the applicant must declare the applicable underlying zoning district for the site. This declared zoning district will determine the permitted uses and the development standards that apply to the property.

The selected zoning district must align with the land use designation identified in *Table 2: Permitted, Conditional and Accessory Uses*, *Table 3A: Supplemental Uses* and the corresponding development standards presented in *Table 2* and *Table 4A & 4B*. Once the final patent is issued by ASLD and the Site Plan or Final Plat is approved, any residential units associated with the parcel will be reflected in the updated Land Use Budget. Refer to *Exhibit 6 - Land Use Budget* for additional information.

TABLE 3A: SUPPLEMENTAL USES
Outdoor Dining Areas are permitted by right in C-2, C-5, BPI and I-1 districts (not CUP).
Retail Liquor Store, without a drive-through component, permitted by right in C-2 and C-5, and exempt from Section 21-503.
Open-Air Markets permitted by right in O-1, C-2 and C-5 districts.
Large Scale Retail permitted by right in C-2 and C-5 (not CUP).
Bed & Breakfast Inn permitted by right in O-1 and C-2 districts.
Manufacturing, processing, refining, testing, and distribution of chemical, petroleum, metal, lumber, and allied products conditionally permitted in I-1 districts, subject to a 1,000' separation from residential districts.

SUPPLEMENTAL USES (CONT'D)
Data centers permitted only in I-1 subject to the issuance of a Special Use Overlay, pursuant to Section 21-680, provided they require a closed-loop system or equivalent low/no water technologies and a minimum separation of 500' from any residential district.
Battery Energy Storage Systems (BESS) are permitted by right in I-1 Districts located in Core 3, provided they are at least (600') from residential property line. Subject to Section 21-500 of the Zoning Code.
Religious Campus and related facilities, including, but not limited to, cemetery, funeral home, school, or parsonage, shall be permitted by right on Parcel 2-J.

TABLE 3B: PROHIBITED USES
Bonding, Companies & Non-Chartered Financial (check cashing)
Car Washes prohibited in C-5, BPI and I-1
Adult Uses
Pawn shops
Tanning Salon, Nail Salon, Barber Shop, Beauty Parlor and similar uses prohibited in BPI and I-1 districts.

3.5. Development Standards

The PIC PCD applies existing development standards from the City of Peoria's Zoning Ordinance based on current zoning districts. Where appropriate, modifications to these standards have been incorporated to better align with the long-term development vision for the site. Detailed development standards for the property are outlined in the PIC Standards and Guidelines Report (*Table 4A & 4B* and *Exhibit 4*).



TABLE 4A & 4 B: DEVELOPMENT STANDARDS

4A: DEVELOPMENT STANDARDS FOR RESIDENTIAL DISTRICTS (2)	SF-1 (1)	SF-2	MF-1	MF-2
	SINGLE FAMILY RESIDENTIAL		DETACHED MULTI-FAMILY	TRADITIONAL MULTI-FAMILY
MINIMUM LOT AREA (SF)	5,000	1,200	N/R	N/R
MINIMUM LOT WIDTH (FT)	45' (3) (4)	20'	N/R	N/R
MINIMUM LOT DEPTH (FT)	100' (3)	N/R	N/R	N/R
MAXIMUM LOT COVERAGE (%)	55%	85%	N/R	N/R
MAXIMUM BUILDING HEIGHT FT	30'	30'	30'	48'/60' (5)
FRONT SETBACK (FT)	20'		20'	20'
SIDE-ENTRY GARAGE	10'	N/R	-	-
FRONT-FACING GARAGE	20'	3' or 20' (6)	-	-
SIDE SETBACK	-	-	20' (7)	20' (7)
INTERIOR SETBACK (MIN/TOTAL FT)	5'/10'	5' (6)	-	-
CORNER SETBACK (FT)	10'	10'	15' (7)	15' (7)
REAR SETBACK (FT)	15'	5'	15'	15'
ACCESSORY BUILDINGS	20'	20'	20'	

TABLE 4A NOTES:

N/R = No Minimum Requirements

- SF-1 development standards, where not modified herein shall be subject to R1-6 standards per the City of Peoria Zoning Ordinance.
- Parcels 1-N, 1-O and 1-P shall have a landscaped buffer of no less than 30' feet along the southern and eastern boundary conditions abutting Pleasant Valley and West Wing.
- Lots within Parcels 1-N, 1-O and 1-P, shall be a minimum of 60' wide and 120' deep.
- On Parcels 1-J and 1-K, no more than 40% of lots within a SF-1 development unit may be a minimum width of 45 feet. A minimum of 60% of lots shall be 53' or greater in lot width.
- MF-2 on parcels within Core 1 are limited to a maximum height of 48 feet. MF-2 parcels within Core 2 and 3 are limited to a maximum height of 60 feet or as depicted in the Loop 303 Height Overlay Area.
- Driveway depth must be less than equal to 3' or greater than 20' as measured from the back of sidewalk, or back of curb if a sidewalk is not provided. Architectural elements, such as pop-outs and overhangs, may encroach into the front building setback up to 3'.
- For condominium and attached housing types only building separation applies.

4B: DEVELOPMENT STANDARDS FOR NON-RESIDENTIAL DISTRICTS	O-1	C-2	C-5	BPI	I-1
	OFFICE AND COMMERCIAL USES (1)			BUSINESS PARK & INDUSTRIAL USES (1)	
MINIMUM LOT/SITE AREA (AC)	N/R	N/R	N/R	N/R	N/R
MINIMUM LOT WIDTH (FT)	N/R	N/R	N/R	80'	80'
MINIMUM LOT/SITE DEPTH (FT)	N/R	N/R	N/R	N/R	N/R
MINIMUM FRONTAGE ON ONE ARTERIAL (FT)	N/R	N/R	N/R	N/R	N/R
MAXIMUM LOT COVERAGE (%)	N/R	N/R	N/R	N/R	N/R
MAXIMUM BUILDING HEIGHT (FT) (2)	36'	48'	60'	60'	72'
ARTERIAL STREET SIDE SETBACK (FT)	15'	15'	30'	30'	30'
OTHER STREET SIDE SETBACK (FT)	15'	15'	15'	20'	20'
INTERIOR REAR/SIDE SETBACK (FT)	N/R	N/R	N/R	N/R	N/R
LEAST SIDE (FT)	-	-	-	20'	0'
TOTAL SIDES (FT)	-	-	-	40'	20'
SETBACK FROM RESIDENTIAL ZONES (FT) (3)	30'	30'	50'	50'	50'

TABLE 4B NOTES:

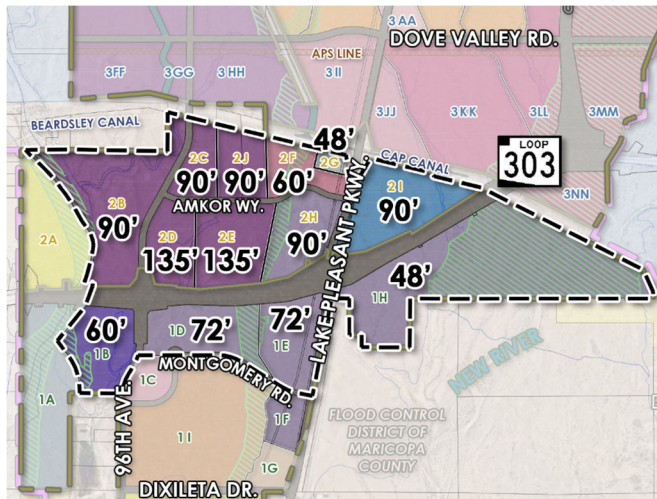
N/R = No Minimum Requirements

- For Parcel 2-1, subject to all applicable building and fire codes; standards applicable for proposals submitted concurrently to the City as a unified mixed use development concept.
- Where the Loop 303 Height Overlay applies, the overlay standards shall govern.
- Setback is applicable when parcel adjoins a residential zoning district outside of this PCD boundary.



3.6. Loop 303 Height Overlay

It is recognized that specified parcels along the Loop 303 Corridor enjoy locational advantages such as visibility, access and proximity to regional drivers. Given their disposition, the PCD provides for an enhanced level of intensity in the form of elevated height allowances. For parcels within the boundary depicted below, the maximum height allowance shall supersede the base zoning requirement.



Loop 303 Height Overlay Area

PARCEL	HEIGHT STANDARDS
1B	60'
1D	72'
1E	72'
1H	48'
2B	90'
2C	90'
2D	135'
2E	135'
2F	60'
2G	48'
2H	72'
2I	90'
2J	90'

3.7. Parking Standards

Development shall meet the minimum parking and loading standards of Section 21-900: Parking and Loading, of the Peoria Zoning Ordinance except as modified below:

A. Off-Street Parking Requirements:

1. Any proposed uses not listed or deemed not comparable to those listed in 21-902 Parking Requirements may submit a Parking Demand Study at the time of site plan approval process to determine off-street parking requirements.
2. For MU designated parcels, a parking study may be considered by the Zoning Administrator to justify an alternative parking standard when shared parking arrangements are proposed.

B. Parking Area Landscaping:

1. Parcels 1-E, 1-F, 2-F and 2-H:
 - a. One (1) parking lot island with raised concrete or decorative curbing shall be provided for every eight (8) parking spaces. In addition, landscape islands shall be provided at the end of each parking row.
2. Parcels 1-B, 1-D, 2-B, 2-C, 2-D, and 2-E:
 - a. One (1) parking lot island with raised concrete or decorative curbing shall be provided for every ten (10) parking spaces, except where covered parking with solar panels are provided, one (1) parking lot island for every eighteen (18) parking spaces may be provided. In addition, landscape islands shall be provided at the end of each parking row.
 - b. Landscaped medians are not required where double rows of parking exceed thirty (30) total spaces.

3.8. Accessory Buildings and Uses

Unless otherwise specified in this PCD, accessory buildings and uses shall conform with Section 21-315 (“Accessory Buildings and Uses”) of the Peoria Zoning Ordinance.

3.9. Accessory Dwelling Units

Accessory Dwelling Units are permitted on any lot or parcel developed as single-family residential pursuant to Section 21-316 (“Accessory Dwelling Unit”) of the Peoria Zoning Ordinance.



3.10. Open Space Requirements

Open space planning and amenities must comply with the applicable standards set forth in the City of Peoria Community Design Guidelines, the Hillside Development Overlay District, the Desert Lands Conservation Overlay, and the City's landscape regulations, except as noted within the PCD Standards and Guidelines Report.

A. Open Space and NOS

The City of Peoria has requirements for the preservation of Natural Open Space (NOS) per the DLCO, and common or active open space, within a residential development (per Peoria Community Design Guidelines). Given the expansive nature and expected duration build out of the Project, preferred natural open spaces (NOS) areas have been identified over protected rivers, washes, and where terrain rises above 15%. These strategic areas are sought to preserve environmentally sensitive areas, recreational amenities, and pedestrian connectivity. Should each core area be divided into smaller parcels as contemplated, the master developer for that core area may satisfy the minimum required open space and NOS acreage through one or more of the following methods/options:

1. Dedication of open space and NOS will be provided in common area tracts, and the total acreage to be dedicated will meet or exceed the minimum acreage of open space and NOS required for the entirety of the project; or
2. Dedication of open space and NOS will occur on an individual parcel basis, which when totaled together, will meet or exceed the total acreage required for open space and NOS; or
3. Dedication of open space and NOS will occur through a combination of common area dedications for the project and individual parcel dedications, which when totaled together, will meet or exceed the total acreage required for open space and NOS.

At the time of the first Site Plan or Preliminary Plat for each Core Area, the master developer shall elect which of the three options identified above shall be utilized to satisfy minimum open space and NOS requirement. Should Option 2 or 3 be selected, the Master Developer shall establish an open space and NOS dedication schedule with the first site plan, which is to be updated with each subsequent parcel development, until such time as the project has met its total open space and NOS obligations. Actual NOS and open space areas for each subarea will be further refined at the time of site plan or preliminary plat application, as applicable.

B. Residential Project Open Space (PCDG)

For individual single-family, multi-family and horizontal mixed-use developments, landscaping and open space must meet or exceed the required amount specified in both the Community Design Guidelines and the Zoning Ordinance. Vertical mixed-use developments are also subject to these same guidelines; however, they must provide a minimum of 10% net of site landscaping area.

3.11. Lighting

Exterior lighting shall comply with Chapter 21 (Section 21-302.F) and Chapter 20 (Sections 20-60 through 20-67) of the City Code and the Peoria Engineering Standards Manual, as applicable.

3.12. Screening, Fencing and Walls

Walls, fences and screening shall conform to the Zoning Ordinance and Peoria Community Design Guidelines, unless otherwise approved through the site plan or preliminary plat process.

3.13. Roadway Standards

Functional classification for Collectors, Arterials and Parkways is designated within the Peoria General Plan (Circulation Map). The specific design and cross-section shall be determined by the City of Peoria Engineering Standards (PESM), or as approved by the City of Peoria Traffic Engineer.



3.14. Design Review Standards

Unless otherwise modified herein, development shall comply with the City of Peoria Community Design Guidelines.

3.15. Landscaping

Except as modified below, development shall comply with the landscaping requirements identified within Section 21-800 of the City of Peoria Zoning Ordinance.

1. The Street Frontage Landscape Buffer identified in Section 21-818.A.1 (“General Landscape Requirements – On-Site Landscape Areas”) shall be a minimum of 20’ in width along all Collector and higher roadway classifications, regardless of use.
2. Local Streets shall provide a Street Frontage Landscape Buffer pursuant to Section 21-818 of the Zoning Ordinance.
3. The PCD shall be exempt from Section 21-819 of the Zoning Ordinance.

3.16. Signs

Unless otherwise modified herein, development shall comply with Section 21-700 of the City of Peoria Zoning Ordinance.

4. Project Implementation

4.1. Project Governance

The standards and regulations outlined in this Planned Community District (PCD) represent the minimum requirements necessary to promote public safety, health, comfort, convenience, and overall welfare. When questions arise concerning the interpretation or application of any part of this PCD, The Zoning Administrator shall be responsible for determining the appropriate interpretation as outlined within the provisions of the Peoria Zoning Ordinance.

4.2. Master and Secondary Planning

The primary objective of the Master Plans is to ensure that infrastructure within the PCD is properly designed and adequately scaled to support

development at the densities and intensities approved for both the individual Development Cores/Parcels and the overall PCD. To this end, the following Master Plans have been submitted with this proposal: Master Drainage Report, Master Water and Wastewater Reports, Master Traffic Analysis, Open Space and Trails Master Plan, Public Facilities Master Plan, Electrical Power Demand Forecast and Transmission Plan, and the Desert Lands Conservation Report.

These Master Plans and Reports provide a conceptual outline of the backbone infrastructure expected for the Project throughout its development lifecycle. It is important to understand that future development plans will need to remain flexible, as the final build-out will be constrained by the available infrastructure to support the PIC.

The PCD Standards and Guidelines report acknowledges these Master Reports and then identifies the responsibilities of property purchasers related to the subsequent planning required for the next phase of development. Factors such as type or intensity of land uses envisioned, or acreage of the purchase may require refinement of the established Master Plans for that specific Development Core/Parcel.

Master Plan Update Process

The initial acquisition of property within a Core Area includes the obligation to update the Master Plans for that specific unit or a portion thereof. These plans must be developed by licensed professionals registered to practice in the State of Arizona. For non-patented parcels, the Master Plans must be updated to reflect the proposal and then submitted to the Arizona State Land Department (ASLD) for review and approval before any formal submission to the City of Peoria. For patented parcels, the developer is required to submit updated Master Plans either prior to, or concurrent with, the necessary site plan (or preliminary plat) application.

Prior to updating the Master Plans, applicants are required to file and participate in a pre-application meeting with City of Peoria. This meeting will serve to review the purpose and scope of revisions needed to the applicable Master Plans. The specific extent of



revisions may vary depending on the size of the land acquired, and the appropriate coverage area.

4.3. Development Parcel Plan Requirements

Upon establishment and approval of the Master Plans, the initial acquisition of property within a Development Parcel includes the obligation to prepare Development Plans for that specific parcel or a portion thereof. At a minimum, the Development Plans should address the following items as stand-alone reports:

4.4. Vehicular Circulation

- Identify the vehicular network intended to serve the parcel, including required right-of-way widths and improvement levels necessary to maintain adequate service levels. At a minimum, the network should address ADOT access, limited parkways, arterial and collector streets. The identified adjacent/backbone roadways are depicted in *Exhibit 4 - Street Circulation Plan*. Alignments and access are conceptual in nature and may be refined from time to time as development occurs.
- Provide typical roadway cross-sections.
- Outline proposed traffic control measures at key intersections of arterial and collector streets.
- Include a traffic signal warrant analysis at applicable intersections.
- Present a conceptual phasing plan for the construction of road infrastructure.
- Highlight any off-site road improvements that may be needed to support acceptable traffic conditions within the Development Parcel and broader Core areas.
- Identify any roadway segments subject to Arizona Department of Transportation (ADOT) access restrictions.

4.5. Drainage

- Refine or update mapping of all off-site watersheds contributing 100-year flows exceeding 100 cubic feet per second (cfs) to the site.
- Identify all relevant balanced and critical drainage basins.

- Document existing upstream drainage infrastructure.
- Highlight areas prone to sheet flow, noting the average water depth.
- Identify any FEMA-designated floodways and floodplains.
- Describe any proposed alterations or encroachments on major drainage patterns.
- Provide layout and description of the drainage systems planned for the site.
- Explain how the proposed development will comply with the City of Peoria's floodplain and erosion hazard standards.

4.6. Water

- Show the locations and sizes of existing major water lines and system infrastructure.
- Identify water pressure zones within and around the Development Parcel.
- Provide locations and specifications for proposed water transmission infrastructure, such as main lines, booster stations, or pressure reducing valves necessary to support the development.
- Specify connection points to the existing water system.
- Indicate locations and dimensions of proposed water line stubs intended to serve internal parcels.

4.7. Wastewater

- Display the size and placement of existing sanitary sewer infrastructure.
- Identify new sewer mains proposed for the area and clarify if these will rely on gravity flow.
- Show proposed connection points to the current sewer system.
- Include the layout and size of any sewer line stubs that will provide service to individual parcels within the Core areas.

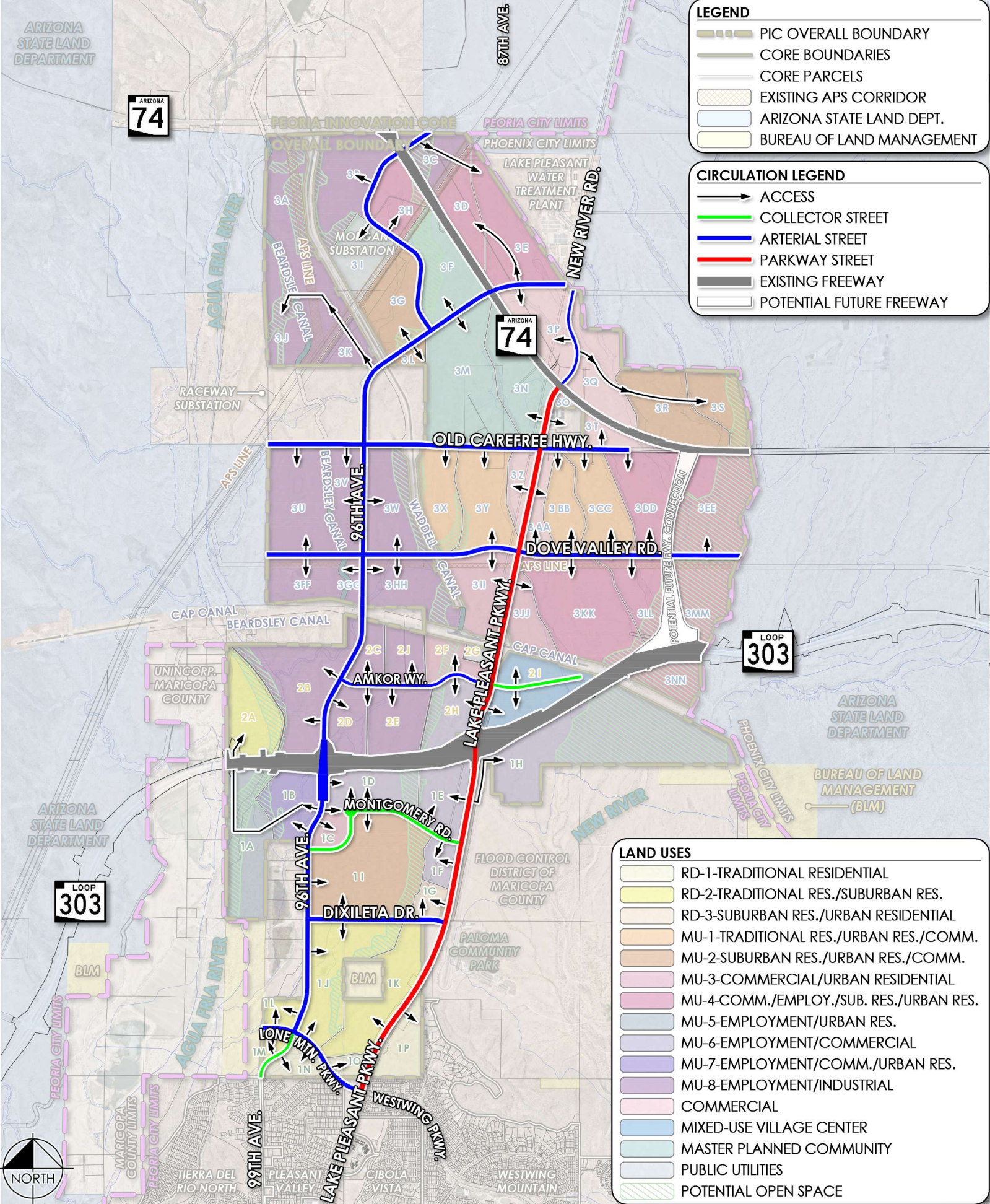
ARIZONA 74

LEGEND

- PIC OVERALL BOUNDARY
- CORE BOUNDARIES
- CORE PARCELS
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT

CIRCULATION LEGEND

- ACCESS
- COLLECTOR STREET
- ARTERIAL STREET
- PARKWAY STREET
- EXISTING FREEWAY
- POTENTIAL FUTURE FREEWAY



LAND USES

- RD-1-TRADITIONAL RESIDENTIAL
- RD-2-TRADITIONAL RES./SUBURBAN RES.
- RD-3-SUBURBAN RES./URBAN RESIDENTIAL
- MU-1-TRADITIONAL RES./URBAN RES./COMM.
- MU-2-SUBURBAN RES./URBAN RES./COMM.
- MU-3-COMMERCIAL/URBAN RESIDENTIAL
- MU-4-COMM./EMPLOY./SUB. RES./URBAN RES.
- MU-5-EMPLOYMENT/URBAN RES.
- MU-6-EMPLOYMENT/COMMERCIAL
- MU-7-EMPLOYMENT/COMM./URBAN RES.
- MU-8-EMPLOYMENT/INDUSTRIAL
- COMMERCIAL
- MIXED-USE VILLAGE CENTER
- MASTER PLANNED COMMUNITY
- PUBLIC UTILITIES
- POTENTIAL OPEN SPACE



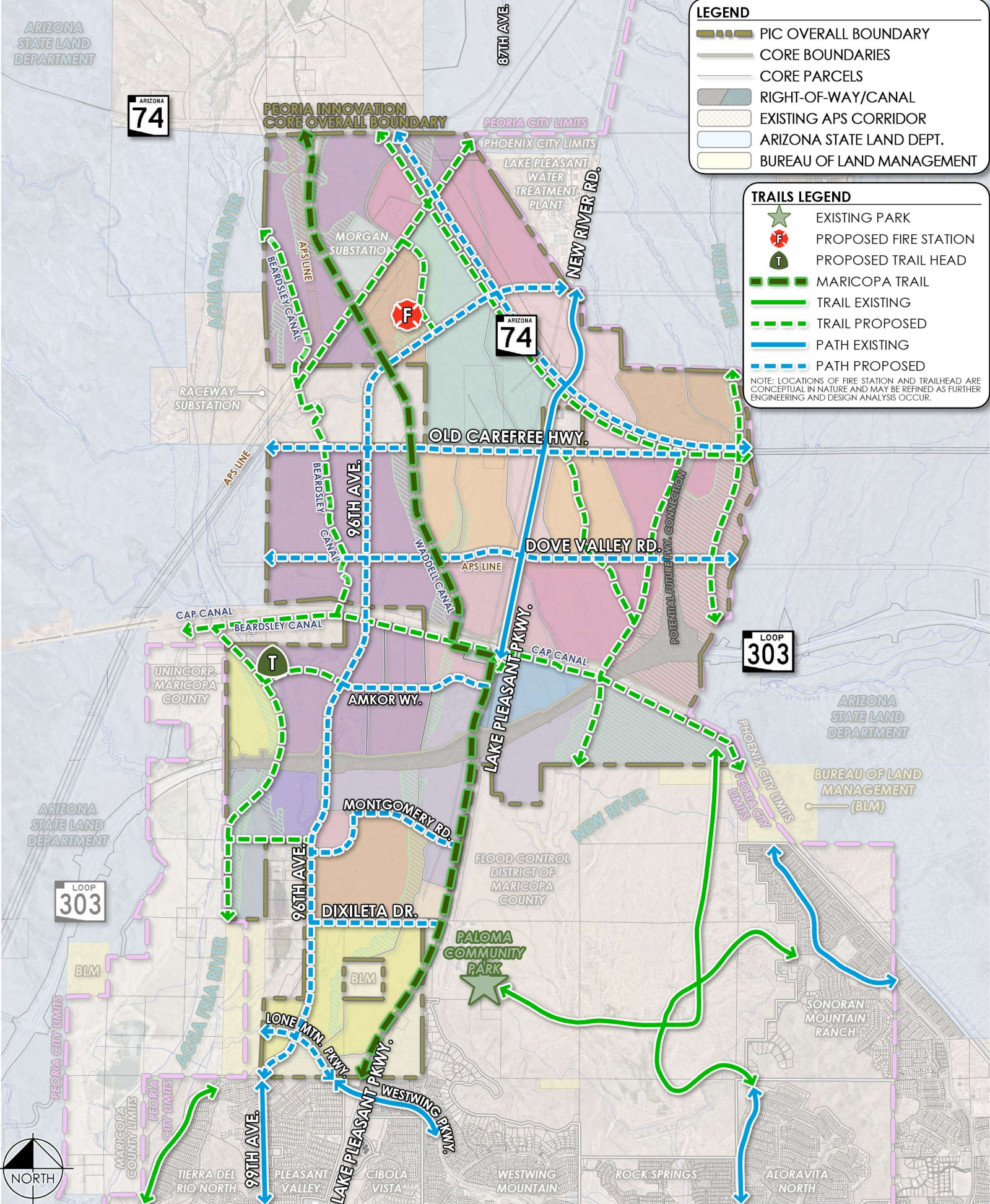
ARIZONA 74

LEGEND

- PIC OVERALL BOUNDARY
- CORE BOUNDARIES
- CORE PARCELS
- RIGHT-OF-WAY/CANAL
- EXISTING APS CORRIDOR
- ARIZONA STATE LAND DEPT.
- BUREAU OF LAND MANAGEMENT

TRAILS LEGEND

- EXISTING PARK
 - PROPOSED FIRE STATION
 - PROPOSED TRAIL HEAD
 - MARICOPA TRAIL
 - TRAIL EXISTING
 - TRAIL PROPOSED
 - PATH EXISTING
 - PATH PROPOSED
- NOTE: LOCATIONS OF FIRE STATION AND TRAILHEAD ARE CONCEPTUAL IN NATURE AND MAY BE REFINED AS FURTHER ENGINEERING AND DESIGN ANALYSIS OCCUR.





4.8. Open Space, Recreation & Trails

- Identify the location and total area designated for primary open space within the Development Cores/Parcels.
- The primary or backbone paths and trails are identified within *Exhibit 5 - Paths and Trails*. In addition, secondary and tertiary pedestrian connections will be identified as each development plan comes in for City review for the Development Cores/Parcels to show how they will connect with the PIC and to Peoria’s broader trail network.

4.9. Public Facilities

- Required public facilities will be integrated into individual Development Cores/Parcels as necessary.
- Final site selection will be coordinated between the developer and the appropriate agency through the City’s development review process.
- Provide schematic representations of potential locations and site areas for fire and police stations.
- Show conceptual placement and size for future school sites.
- Identify schematic locations and approximate areas for City parks.

4.10. Unspecified Land Uses

Any land use not explicitly identified in this PCD is considered prohibited. However, it is the responsibility of the Zoning Administrator to determine whether an unlisted use is analogous and/or similar to a listed use based on its physical and operational characteristics. If deemed comparable, the unlisted use will be treated in accordance with the regulations applied to the comparable listed use.

4.11. Development Regulations and Standards Not Listed

In instances where this PCD does not specify development regulations or standards, those of the City of Peoria Zoning Ordinance, and the Community Design Standards shall apply, depending on the situation. For issues not addressed in this document, the City’s current adopted regulations at the time of

the development application will be used unless otherwise specified.

4.12. ASLD/Purchaser Role

The ASLD retains exclusive authority to determine when and how Trust land within this PCD is made available for sale. While ASLD maintains ownership of land within the PCD, any landowner other than ASLD must obtain written authorization from ASLD before requesting a Zoning Administrator interpretation or amendments to this PCD.

If the City receives such a request without ASLD approval, the City must notify ASLD.

- Until all State Trust land in the PCD is patented, ASLD will remain actively involved in land use decisions. This ongoing involvement is intended to ensure that development is consistent with this PCD and beneficial to both current and future phases. ASLD has the authority to:
- Review development packages, improvement plans, subdivision plats, and related documents prior to submission to the City or any other reviewing agency.
- Require an ASLD Planning Authorization Letter with all City submittals.
- Review and approve development proposals when the City requests substantial changes to submitted plans.
- Approve proposed land uses, PCD amendments, and any applications affecting development standards prior to formal submission to the City of Peoria.
- Approve all Master Plans related to the Secondary Planning process, which must also include an ASLD Planning Authorization Letter upon submittal.
- Assign residential densities and allocate dwelling units to specific sale parcels.
- Define the sequence, size, and location of property sales within the PCD.



4.13. Land Use Budget and Planning Allocation

The State Land Commissioner (or a designated representative) is responsible for determining permitted residential densities for each parcel prior to auction. All applications for site plans or preliminary plats must include documentation showing the Commissioner's approval of the density allocation—this requirement continues until the last parcel is patented.

The City of Peoria will maintain a running total of entitled residential units in a Land Use Budget (see *Exhibit 6*). This table will be updated as site plans and final plats are approved. If a development plan fails to result in patenting by ASLD, the associated units will be returned to the available unit pool in the Land Use Budget.

4.14. Amendments

Any amendments to this PCD must follow the process outlined in Section 21-613 of the City's Zoning Ordinance. The Zoning Administrator may approve minor administrative changes to the boundaries of Development Parcels, particularly when adjustments are necessary due to final roadway alignment or other development limitations. The total allowable residential density is controlled through a combination of the overall unit cap and the underlying zoning standards applicable to each parcel. This PCD does not establish parcel-specific maximum density requirements. No amendments to this PCD are permitted without the explicit consent of ASLD while Trust Land remains under their ownership.

5. Infrastructure

Master Plan updates or addendums, as specifically conditioned, for individual core areas will be required as needed:

Master plans for the Peoria Innovation Core shall be updated as necessary, as determined by the Zoning Administrator and City Engineer, with any

subsequent Site Plan or Preliminary Plat submittals within a CORE.

Right-of-way and easement dedications shall occur in accordance with City requirements and the findings of any approved master plans, or addendums thereof.

5.1. Street Circulation

For a detailed summary of existing and proposed infrastructure conditions related to street circulation, please refer to the Master Traffic Impact Analysis prepared by Y2K & Kimley-Horn (March 2026 Date).

5.2. Water and Wastewater

For a comprehensive overview of existing and planned water and wastewater infrastructure, please see the Water & Wastewater Report authored by Kimley-Horn (March 2026 Date).

5.3. Drainage

For information regarding current and anticipated drainage infrastructure, consult the Master Drainage Report by Kimley-Horn (March 2026 Date).

5.4. Electrical Power Demand & Transmission

For information regarding current and anticipated electrical power demand & transmission, consult the Electrical Master Plan by Kimley-Horn (March 2026 Date).



EXHIBIT 6 – LAND USE BUDGET

DEVELOPMENT AREA			RESIDENTIAL UNITS APPROVED TO-DATE	DATE OF UPDATE
CORE	MAX DU	PARCEL		
1	3,500	A		
		B		
		C		
		D		
		E		
		F		
		G		
		H		
		I		
		J		
		K		
		L		
		M		
		N		
		O		
P				
TOTAL				

DEVELOPMENT AREA			RESIDENTIAL UNITS APPROVED TO-DATE	DATE OF UPDATE
CORE	MAX DU	PARCEL		
2	800	A		
		B		
		C		
		D		
		E		
		F		
		G		
		H		
		I		
TOTAL				

DEVELOPMENT AREA			RESIDENTIAL UNITS APPROVED TO-DATE	DATE OF UPDATE
CORE	MAX DU	PARCEL		
3		A		
		B		
		C		
		D		
		E		
		F		
		G		
		H		
		I		
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		R		



3 (CONT'D)	6,700	S	
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		X	
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LL			
MM			
NN			
TOTAL			

NOTES

(1) Units associated and entitled with any proposed development plan that fail to transfer out of ASLD's ownership (issued a land patent) shall be returned to the Land Use Budget Table following notice to the city by ASLD.